

7 COMMUNITY DESIGN

The focus of the Specific Plan design concept is a physical organization of the plan area that will establish harmonious external relationships to the surrounding neighborhoods and complementary internal relationships among the commercial, residential, recreational, and open space areas of the Specific Plan area. This can be accomplished using the following design approaches:

- ❖ Physical connections and view linkages that integrate the Plan area and give it a cohesive sense of place.
- ❖ Human scale, diverse architecture consistent with the eclectic design and modest size typical of Larkspur's built environment.
- ❖ Extension of the Magnolia Avenue storefront pattern to enhance Downtown's small town character.
- ❖ Streets and blocks that, to the extent possible within the confines of the available space and other design objectives, replicate a traditional rectangle-shaped interconnected grid pattern.
- ❖ Pedestrian-oriented narrow streets (and alleys where possible) to slow traffic, increase livability, and create a village neighborhood character.

- ❖ Bicycle and pedestrian linkages to Downtown, nearby schools, the regional bikeway system, and open space in the planning area.

7.1 DESIGN OBJECTIVES

The City intends to focus the design effort on three major physical elements:

- ❖ Significant physical components that can be used to provide a unified, readily apparent, and positive image for the plan area (Figure 7-1).
- ❖ Key relationships with immediate Plan-area surroundings that should be preserved and strengthened (Figure 7-2).
- ❖ Protection and appropriate use of natural and scenic resources.

The following objectives will direct the City's efforts.

Objective D-1 A unified, readily apparent and positive image for the plan area. This image will be of a gathering place that features design elements from Downtown's historic architecture, defining elements of pre-1940 residential neighborhoods, and creekside environmental resources.

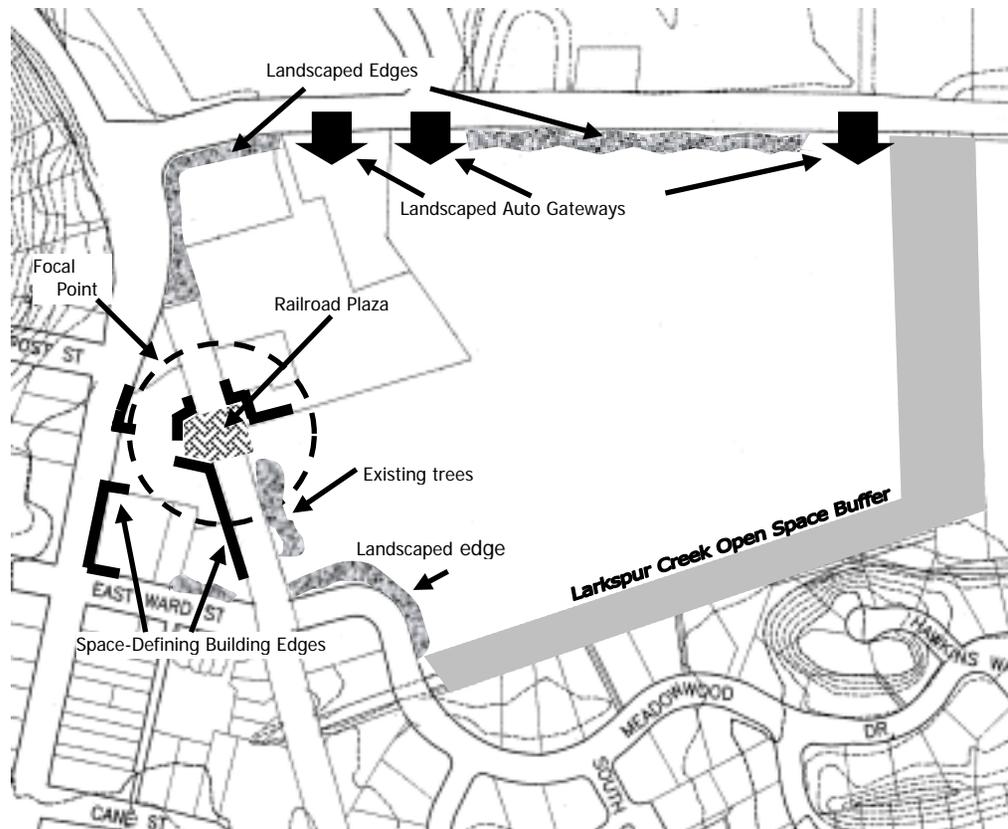


Figure 7-1 Significant Physical Components

Objective D-2 Functional and visual integration among the three subareas of the Plan area and convenient pedestrian and bicycle linkage to Downtown, Piper Park, Hall Middle School, Redwood High School, and neighboring residential areas.

Objective D-3 The storefront development pattern extended along the Magnolia Avenue frontage of the planning area with special emphasis on Downtown’s historic design character and pedestrian scale.

Objective D-4 Traditional residential neighborhood design reflecting the scale and diversity of Larkspur’s original neighborhoods.

Objective D-5 Major natural elements protected, enhanced, and integrated (including the creek, creekside vegetation, and heritage trees) with a system of pedestrian and bicycle routes to visually and functionally unify the Plan area.

7.2 GENERAL DESIGN POLICIES

The following policies apply throughout the Specific Plan area. These policies are intended to provide a framework for achieving the design objectives for the Plan area.

Policy D-1. Traditional Design Approach. The overall approach to design shall reflect traditional downtown and residential neighborhood design in which walking is the principal means of getting around, and buildings are designed at a human scale and relate to the street and sidewalk.

Policy D-2. Integration. The three principal subareas of the Specific Plan shall be integrated into a coherent whole by an organizational framework of physical and visual connections as well

as specific design approaches. The physical connections shall include, but not be limited to, roadways, multipurpose bicycle and pedestrian pathways, and open space linkages. Visual connections will include visual corridors through the Plan area providing vistas of Mount Tamalpais as well as views of key landmarks within the Plan such as the railroad buildings. Specific design approaches could include building architecture themes, landscape planting patterns, and details such as street names.

Policy D-3. Focal Point. Coordinate the design of the contiguous portions of Subareas 1, 2, and 3 to create a sense of place and a focal point for Downtown.

Policy D-4. Linkages to the Surrounding Area. Functionally and visually link major areas and destinations within the Plan area to Downtown, Piper Park, Hall Middle School, Redwood High School, and neighboring residential areas.

Policy D-5. Views. Protect and enhance views of the railroad buildings, Larkspur Creek, and Mt. Tamalpais as a means of visually linking the Plan subareas with each other and the larger community.

Policy D-6. Pedestrian and Bicycle Orientation. Encourage more travel by foot and bicycle by integrating the design of multipurpose pedestrian/bike paths with adjoining development to create an environment that is safe, attractive, and convenient for pedestrians and bicyclists.

Policy D-7. Building Design. The design of buildings should incorporate the best architectural traditions of Larkspur in terms of

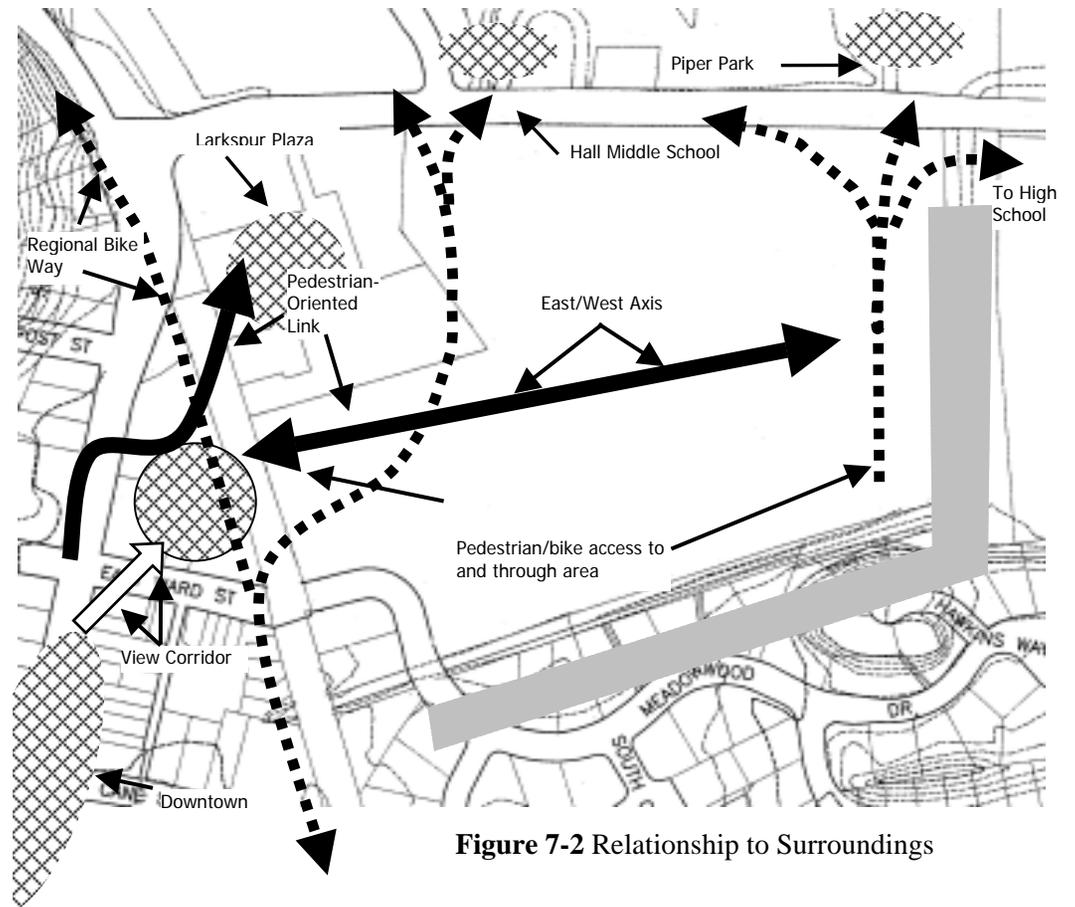


Figure 7-2 Relationship to Surroundings

scale, building materials, and forms in a manner that expresses the evident historic progression within the community. (See also Policy D-25.)

Policy D-8. Landscape Character. Provide an informal, naturalistic landscape throughout the Plan area consistent with the prevailing landscape character of Larkspur.

Policy D-9. Creek Resources. Protect and enhance the natural resource and scenic values of Larkspur Creek. To minimize soil erosion and other secondary impacts on wildlife by pedestrians and cyclists, no bikeways or footpaths shall be constructed within the Larkspur Creek buffer area (See Standard D-63 for width of the buffers). Permanent fencing shall be installed along the outside edge of the buffer to discourage people and their pets from entering the restored creekside habitat. Passive community use may be established near the Larkspur Creek corridor but not within the buffer area.

Policy D-10. Open Space Network. Integrate the area's major natural elements (the creek, creekside vegetation, and heritage trees) with a system of pedestrian and bicycle routes to define the entire area visually and to tie the individual parcels together into a site-wide open space network. However, prohibit pedestrian and bicycle paths from being located within the Larkspur Creek buffer and require that permanent fencing be installed along the outer edge of the buffer to discourage people and their pets from entering the restored creek side habitat. Pedestrian and bicycle routes may be established near the Larkspur Creek corridor but not within the buffer area.

Policy D-11. Historic Railroad Buildings. Retain the former railroad station and warming house buildings in their present locations and provide an appropriate setting that allows them to

serve as a focus for future development and a readily apparent link to the city's origin.

Policy D-12. Railroad Right-of-Way. Design the limited development permitted in the railroad right-of-way (i.e., parking, pedestrian/bicycle path, railroad building plaza and landscaping) to retain the context of a railroad line, including, where possible, such elements as the passenger platform and the track alignment. Pedestrian and bicycle safety shall be a priority in any planning and/or designs for the right-of-way area.

Policy D-13. Flooding and Water Quality. Design and construct buildings, parking, roads and utilities to minimize water runoff problems such as flooding and water quality degradation.

Policy D-14. Green Building. Green Building design, demolition, construction, and operation can have a significant positive effect on energy and resource efficiency, waste and pollution generation, and the health and productivity of a building's occupants over the life of a building. The City strongly encourages applicants to employ Green Building site and design methods that reduce energy and increase resource efficiency consistent with the County's point system, until such time as the City of Larkspur develops and adopts its own.

Policy D-14A. Noise and Lighting. New commercial construction or change of use shall be reviewed either at the time of design review or, for existing buildings, business license approval to ensure compatibility with adjacent districts or uses through the following measures: noise shall not exceed standards outlined in the City's Noise Ordinance; no continuous, frequent or repetitive odors shall be permitted which are perceptible on or beyond the adjacent property lines; lights shall be of the minimum illumination necessary for a given application and shall be directed downward and shielded at lot

lines so as not to be directly visible from an adjoining residential district.

7.3 SUBAREA 1 DESIGN POLICIES AND STANDARDS

The following section sets design policies and standards for Subarea 1, including policies for certain individual parcels such as the City parking lot and the American Legion property.

The essential elements of the design policies for Subarea 1 are to enhance the sense of entry to Downtown, promote Downtown's historic character, maintain and enhance the pattern of Downtown store frontage, establish the railroad buildings as a focal point, and to create strong links to Subareas 2 and 3. Development within the Downtown Historic District is subject to the review and recommendations of the Historic Preservation Board. The Board's review shall be guided by the following design policies and standards for Subarea 1.

Policy D-15. Storefront Downtown Standards. The development standards outlined in Chapter 4, Land Use, and the Storefront Downtown Zoning District regulations (Chapter 18.41 Larkspur Municipal Code) shall apply in Subarea 1. Where the Specific Plan policies and standards differ from the standards in Chapter 18.41, the Specific Plan policies and standards shall supercede.

Building Location

Policy D-16. Downtown Storefront Pattern. Buildings shall be designed and located to extend and maintain the continuity of the Downtown storefront pattern along Magnolia Avenue. The Storefront

Downtown Zoning District yard requirements for main structures and accessory buildings (sections 18.41.090 and 18.41.100 Larkspur Municipal Code) shall apply in Subarea 1.

Policy D-17. Buildings Without Magnolia Avenue Frontage.

Where buildings adjoin community-serving outdoor spaces or pedestrian routes, but do not front on Magnolia Avenue, they shall be designed and located to provide spatial definition, human scale, and visual interest.

Policy D-18. View to Railroad Buildings. A view corridor shall be provided to the railroad buildings from Magnolia Avenue. The view corridor should be sufficient width and in an alignment to allow pedestrians on Magnolia Avenue to readily identify the railroad buildings as a landmark and visual focal point. The preferred alignment of this view corridor is from the sidewalk at the northeast corner of the intersection of Magnolia Avenue and Ward Street. Other alignments may be considered to implement Specific Plan goals and policies.

Standard D-1. Build-to-line. In order to maintain and extend the storefront pattern of Downtown, buildings fronting on Magnolia Avenue shall be built to the property line along frontages with a mandatory build-to-line designation. Alternatively, they shall be built to within 5 feet of the property line when the setback is designed to serve as an extension of the public sidewalk.

Building Heights

The Specific Plan's building height standards address the following policies.

Additional 2'6" permitted for code-required parapet

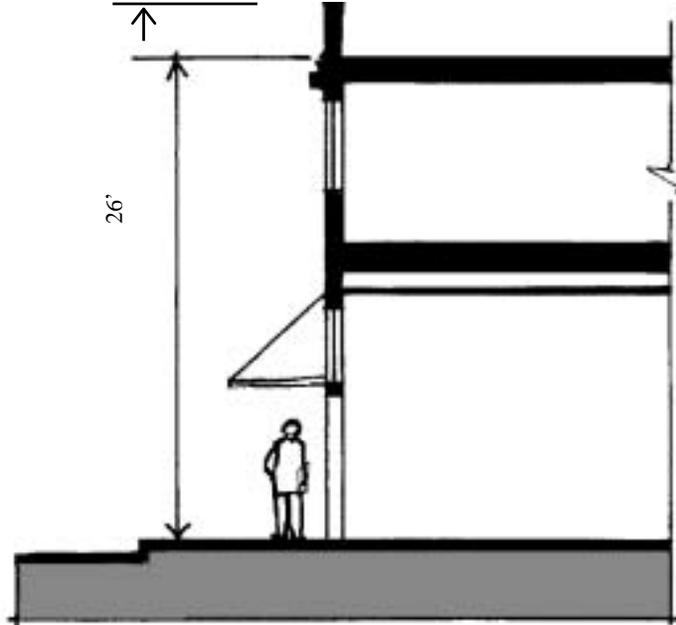


Figure 7-3 Flat Roofs along Magnolia Avenue

Policy D-19. Height and Scale. Allow construction of buildings to a height which, together with existing buildings along the west side of Magnolia Avenue, will create a well defined, human-scale street space for the northernmost block of Downtown. Promote modulation and articulation of buildings to replicate the scale and variation of existing Downtown facades by permitting a combination of two- and three-story construction.

Policy D-20. Height Allowances. A height allowance of 26 feet above ground level is permitted with an additional 2 feet and 6 inches permitted for parapets that may be required by the Building Code. The Planning Commission may allow additional height up to 36

feet (see Standard D-4) subject to making findings that the proposed height(s) promote the following:

1. The proposed height allowance together with existing buildings along the west side of Magnolia Avenue, will create a well defined, human-scale street space for the northernmost block of Downtown.
2. The proposed building heights will protect solar access to community-serving outdoor spaces.
3. The proposed heights will minimize site coverage and provide for usable, attractive community-serving open space adjacent to the retained railroad buildings.

Policy D-21. Magnolia Avenue Frontage Height Standards.

The following standards shall apply to buildings fronting on Magnolia

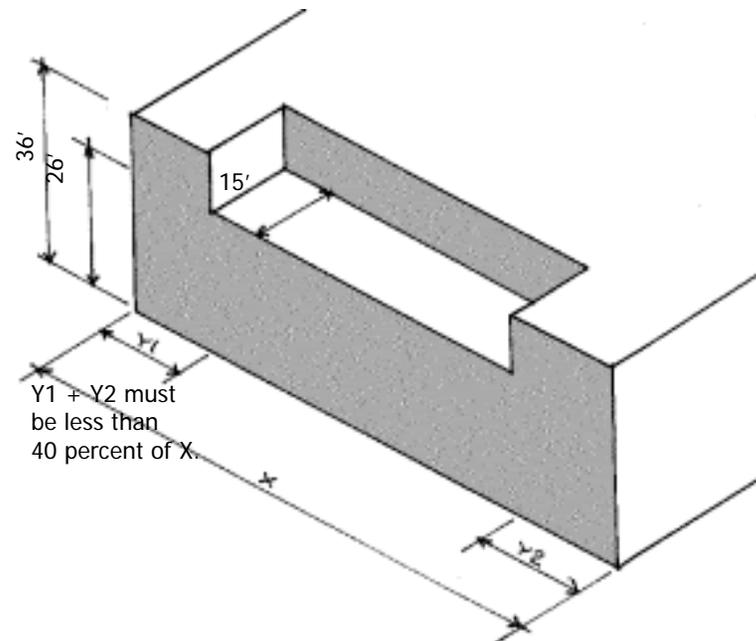


Figure 7-4 36-Foot Allowance along Magnolia Avenue

Avenue to allow varying types of roof construction, including flat-roof buildings with parapet walls and buildings with pitched roofs.

Standard D-2. Minimum Height. A minimum street facade height, inclusive of parapet wall, of 20 feet is required to maintain continuity and promote consistency in scale with existing Downtown storefronts.

Standard D-3. Base Elevation. All facade heights facing Magnolia Avenue shall be measured from an average existing sidewalk elevation of 21 feet above sea level.

Standard D-4. Flat Roof construction. For flat-roof construction, a height of 26 feet above the existing sidewalk elevation is permitted. Additionally, as much as 40 percent of a building's facade frontage along Magnolia Avenue may exceed 26 feet with Planning Commission approval, but may not exceed an absolute height of 36 feet. Any other portion of the building exceeding 26 feet must be set back a minimum of 15 feet from the Magnolia Avenue right-of-way and may not exceed the absolute 36-foot height limit (see Figure 7-4).

Standard D-5. Parapet Walls. Code-required parapet walls may exceed the 26- and 36-foot height limits established above.

Standard D-6. Pitched Roof Construction. Buildings with pitched roofs may exceed the 26-foot height, but may not be higher than 36 feet limit, subject to Planning Commission approval (see Policy D-20) and provided that beyond a height of 26 feet at the property line the building does not project beyond a 45-degree skyplane, as shown in Figure 7-5. Projecting dormers are exempted, provided the face of the dormer is set back at least 5 feet from the front property line and the combined width of the dormers does not exceed 40 percent of

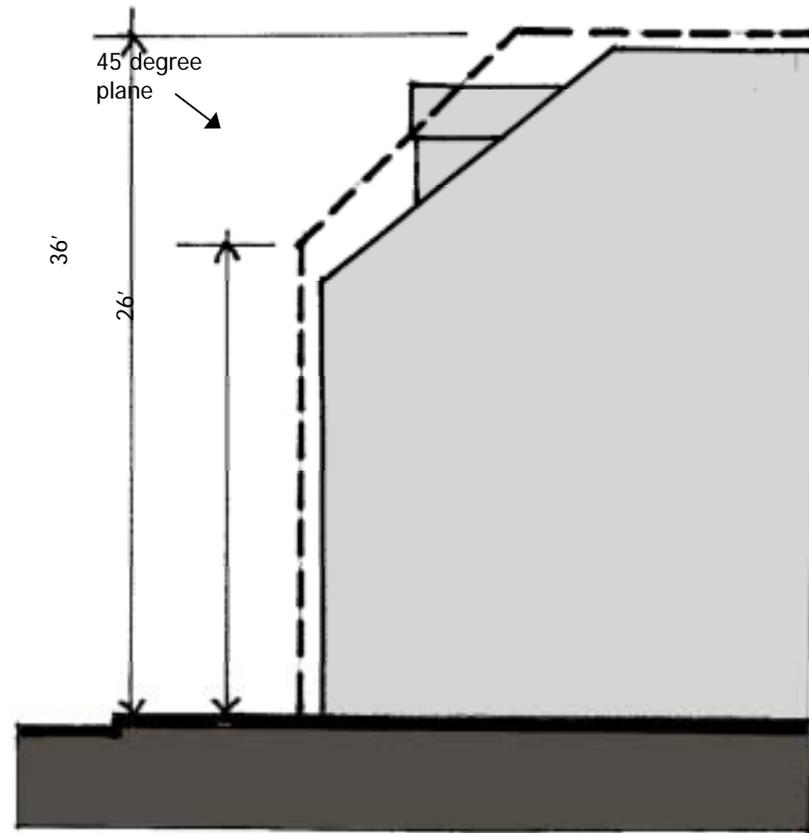


Figure 7-5 Sloping Roofs along Magnolia Avenue

the total width of the associated roof. Walls and railings not exceeding 42 inches in height that enclose a deck or roof terrace are also excluded.

Pedestrian-Oriented Frontage

Policy D-22. Pedestrian-Oriented Frontage. Maintain the existing character of Downtown and encourage walking between Downtown, Larkspur Plaza, and nearby residential areas by providing an attractive, pedestrian-scaled setting along major pedestrian circulation routes.

Policy D-23. Pedestrian-Oriented Frontage Design

Standards. The following standards shall apply to building frontages along Magnolia Avenue pedestrian circulation routes.

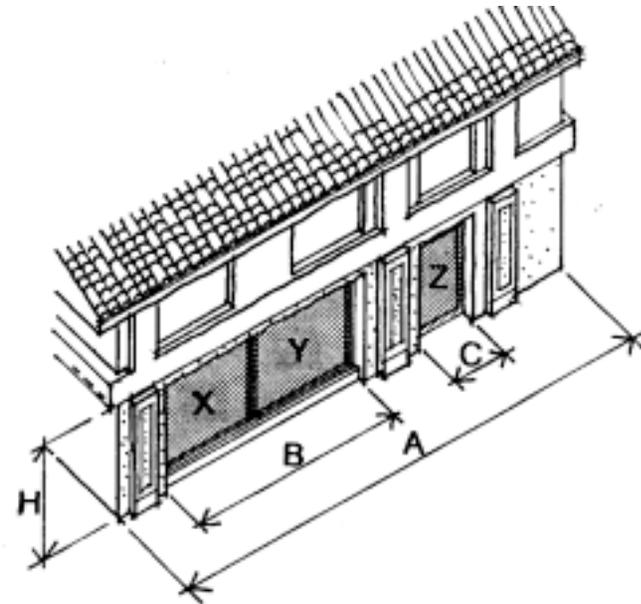
Standard D-7. Ground-Level Facades. The major portion of the ground-level facade along Magnolia Avenue shall be glazed with clear glass in accordance with the formula shown in Figure 7-6.

Standard D-8. Building Entrances. One or more direct entrances shall be provided from adjacent public exterior space to each establishment occupying interior space.

Standard D-9. Pedestrian-Scale Details. Details shall be incorporated into ground-level facades on pedestrian-oriented frontages. Desirable elements include, but are not limited to, the following:

- ❖ Awnings;
- ❖ Windows with muntins and mullions, resulting in small paned windows;
- ❖ Wood window and door casings;

- ❖ Materials such as brick, wood shingles, and vertical or horizontal wood siding that have human-scale dimensions
- ❖ Signage located and sized to be viewed primarily by pedestrians;
- ❖ Projecting or recessed windows and entries;
- ❖ Modulation of the facade to maintain a storefront scale and appearance similar to that found elsewhere along Magnolia Avenue;



Areas X+Y+Z must equal at least 50 percent of the ground-floor wall area of the street-facing building facade (i.e., HxA).

Figure 7-6 Street-Level Window Requirements

- ❖ A cornice or belt course to delineate the ground floor from upper floors.

Standard D-10. Structured Parking Screening. To avoid a barren appearance along community-oriented spaces and routes, structured parking should be screened from the street and pedestrian ways by pedestrian-oriented uses or screened with ornamental grills or other architectural devices so that it does not interrupt or detract from the appearance of ground level and pedestrian-oriented frontages.

Standard D-11. Structured Parking Lighting. All interior illumination sources within parking structures shall be screened so as not to be directly visible from the exterior.

Standard D-12. Magnolia Avenue Pedestrian Amenities. Pedestrian-oriented amenities should be employed to enhance the street and sidewalk setting, including but not limited to the following:

- ❖ Bollards along the road edge to visually separate pedestrians and automobiles.
- ❖ Small-scale street trees that will not block views to Mt Tamalpais or exceed a height of 18 feet at maturity with (or without) appropriate pruning.
- ❖ Awnings and/or canopies along the building facade to create a pedestrian-scale street space.
- ❖ Uniform modular paving materials like brick or concrete pavers. Variations in paving should be limited to special locations, such as bus stops or the entrance to a plaza.

Architectural Design

Due to the location of Subarea 1 at the northern gateway to Downtown Larkspur and within the City's Historic Preservation District, the architectural design of new structures is of major community importance. The following policies and standards address this concern. Policy D-24 emphasizes the potential of new development for reinforcing the sense of entry to Downtown. The standards for historic context are consistent with the U.S. Secretary

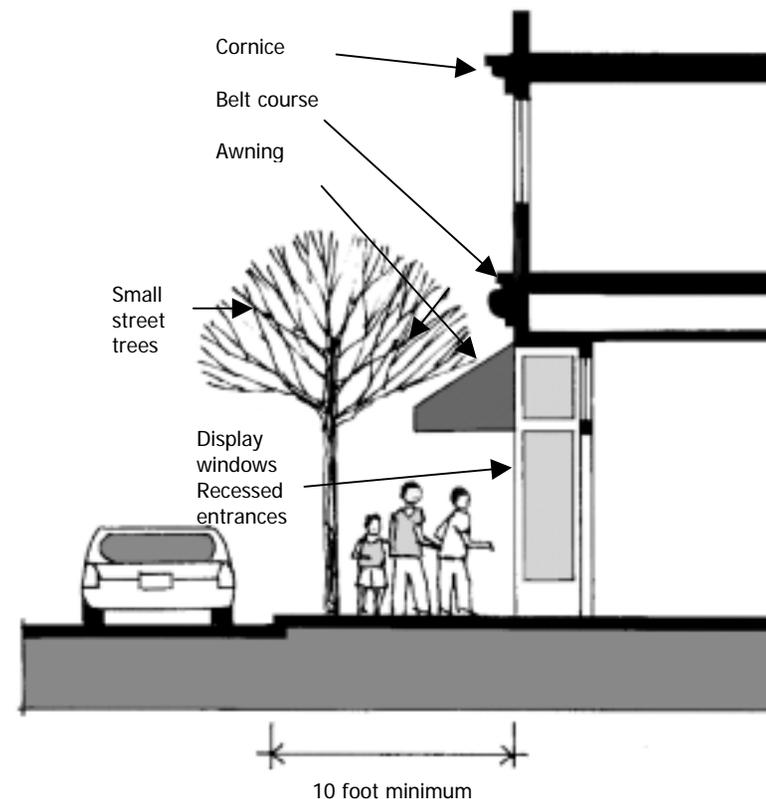


Figure 7-7 Magnolia Avenue Frontage: Human-Scale Design Features

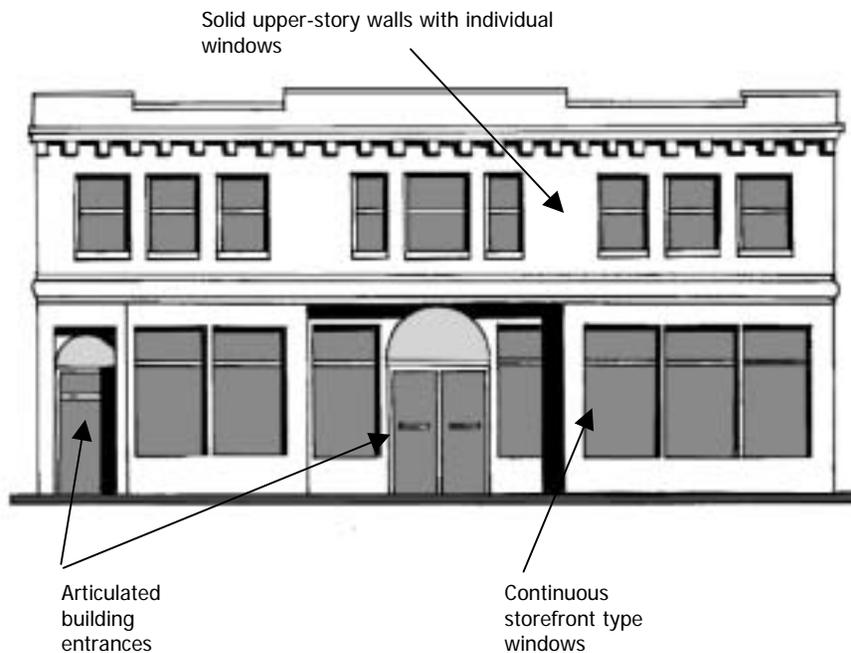


Figure 7-8 Building Facade Guidelines

of Interior's standards, the intent of which is to retain the evident historic progression of community development and allow each building to be recognized as a physical record of its own time, place and use. In keeping with this intent, the Secretary of the Interior's standards require that new work be differentiated from old, but that it be compatible with the historic materials, features, size, scale and proportions of the older structures.

Policy D-24. Northern Entry to Downtown. Buildings in Subarea 1, especially at the intersection of Magnolia Avenue and the driveway into Larkspur Plaza, shall be designed to create a sense of entry into Downtown.

Policy D-25. Architectural Standards for Historic Context.

The design of new buildings and remodeling of existing structures shall retain the physical pattern of the historic progression of community development in Downtown and allow each building to be recognized as a physical record of its own time, place and use. New work shall be differentiated from old, but shall be compatible with the historic materials, features, size, scale and proportions of the older structures. The following architectural standards shall be followed in Subarea 1 to maintain and enhance the historic context of Downtown.

Standard D-13. Replication Discouraged. The architecture of the new buildings should not attempt to replicate a specific historic style.

Standard D-14. Ground-Floor Level Differentiation.

Ground-floor space should be differentiated from upper-floor space and designed as storefronts to maintain consistency with storefronts along Magnolia Avenue and to impart a pedestrian scale. Ground-floor space should have a predominantly glazed surface providing views into the building interior. Upper-floor portions of the building facade should appear as a solid mass, with smaller windows appearing as openings or voids which create a consistent pattern of shadows and which indicate a change in interior use between the ground and upper floor(s). Figure 7-8 is an illustrative example of this standard. (See also Standard D-6 and Figure 7-5.)

Standard D-15. Building Facades Exposed to Public View.

Building facades exposed to public view should either have a parapet or a prominent roof slope. To maintain the prevailing scale of Downtown storefront facades, the major portion of the latter roofs should slope back from the face of the building.

Figure 7-9 is an illustrative example of this standard. (See also Policy D-21 and Figure 7-3.)

Standard D-16. Façade Articulation. Building facades should be articulated so as to provide a storefront module similar to that found elsewhere in the Downtown. The use of a series of distinctly different facades and styles is inappropriate. A consistent, unified architectural appearance should be maintained for each separate building.

Standard D-17. Materials and Colors. Materials should be compatible with those prevailing throughout the historic district. Wood shingles (painted or stained), horizontal wood siding, high-quality (smooth or slightly textured) stucco, and ceramic roof tiles are appropriate materials. Highly finished materials and/or reflective materials such as reflective glass and metal surfaces and rusticated materials such as rough-sawn wood and uncut stone are inappropriate. Building colors can be relatively bright, and may include earth tones and pastels.

Standard D-18. Railroad Station and Warming House.

The original character defining features of the railroad station and warming housing buildings should be documented, to the extent possible, and restored using the following sequence of action.

1. Protect and maintain those features that are extant and in good condition.
2. Repair those features that have been damaged or covered by other construction using the least amount of intervention possible. Do not replace building features that can be repaired.

3. Replace missing historic features using the following order of priorities:

- ❖ Recover, if possible.
- ❖ If unrecoverable, reproduce using photographs or other documentation.
- ❖ If documentation is inadequate, design a new feature that is compatible with the remaining character-defining elements of the building. The new feature should be clearly differentiated to avoid creating a false historical appearance.

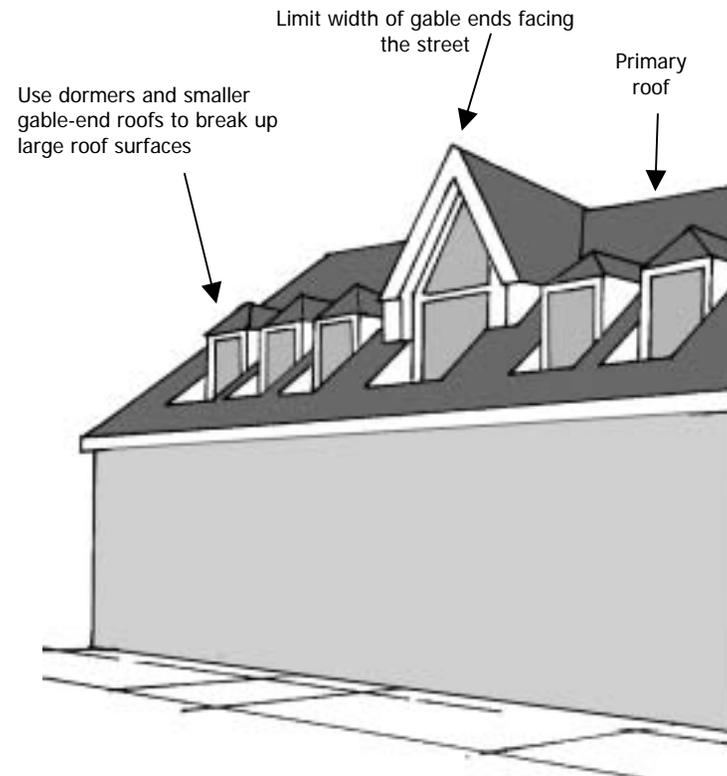


Figure 7-9 Sloping Roof Design

Vehicular Access

Policy D-26. Street Connections. Ingress and egress by motor vehicles should be located and designed to minimize impacts to adjoining streets.

Policy D-27. Driveway Locations. Points of ingress and egress shall be located so as to minimize the potential for conflict with pedestrian circulation and to avoid disrupting the pattern of storefronts along Magnolia Avenue.

Policy D-28. Porte-Cochere Permitted. A porte-cochere for a hotel may be permitted on Magnolia Avenue.

Policy D-29. Vehicle Circulation in the Railroad Plaza. Vehicular circulation within a plaza at the railroad buildings may be allowed if limited solely to hotel-serving functions such as guest registration or access to subterranean hotel parking. Pedestrian and bicycle safety shall be a priority.

Pedestrian/Bicycle Circulation

It is envisioned that Subarea 1 will serve as a major junction of community pedestrian and bicycle routes. It is crucial, therefore, that the design of community-serving outdoor spaces be closely coordinated with those of adjoining parcels to address the requirements of pedestrians and bicyclists. The following policies will promote this end.

Policy D-30. Magnolia Avenue/Railroad Building Connection. A direct pedestrian connection shall be provided from Magnolia Avenue to the railroad station buildings.

Policy D-31. Railroad Right of Way Bikeway and Parking. A Class 1 bikeway shall be provided in the former railroad right-of-way.

Parking may also be installed but shall be limited to one side of the right-of-way in order to preserve the context of a railroad line. Design the bikeway and parking to retain the context of a railroad line, including, where possible, such elements as the passenger platform and evidence of the track alignment. Figure 7-10 illustrates one possible design for parking and a Class 1 bikeway in the railroad right-of-way.

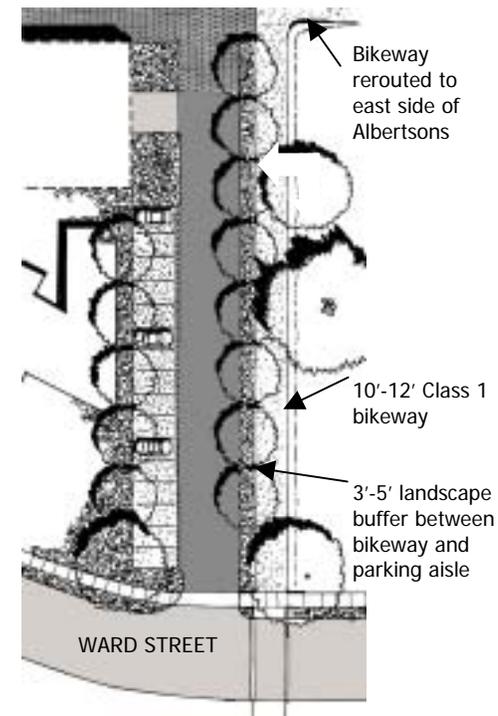


Figure 7-10 Illustrative Only- Maintaining the regional bikeway in the former right-of-way limits parking to one row.

Railroad Plaza

Policy D-32. Railroad Plaza. Specific Plan land use policies require that a centrally located and publicly oriented open space should be provided adjoining the retained railroad buildings. The space should be designed to serve as a focus and amenity for adjacent commercial and residential uses and as a setting for community-oriented activities and events.

Policy D-33. Railroad Plaza Design. To accommodate community-oriented uses, the majority of the area should remain unobstructed and paved with durable and easily maintained materials. Suitable paving materials include brick, asphalt, concrete, tile pavers, and poured concrete with an architectural finish. Use of plant materials for groundcovers should be limited, and trees should be used primarily to define entrances and major circulation routes. Similarly, the use of raised planters should be minimized and limited to peripheral areas so as not to impede use of the area for community events.

Policy D-34. Plaza Landscaping. The landscape design of the plaza should acknowledge and commemorate the original alignment of the railroad right-of-way.

City Parking Lot

The City intends to retain use of this parcel for public parking unless the sale, lease or exchange of the property facilitates construction of other desired public facilities, such as a library or expanded Downtown-serving public parking. In the event the latter course of action is taken and the site is made available for building construction, the following policy and standard shall be applied in

addition to the Subarea 1 Land Use Policies and Standards in Chapter 4 of this Plan.

Policy D-35. Standard for Northeast Corner of Magnolia Avenue and Ward Street. The following standard shall be applied to any freestanding building on the City Parking lot site.

Standard D-19. Northeast Corner of Magnolia Avenue and Ward Street. Any new structure should be set back a sufficient distance to create a small plaza space at the corner.

The American Legion Property

The American Legion facilities are an assemblage of three older structures, two of which date to the early 1900s. The following policies apply to this parcel.

Policy D-36. New Construction on the American Legion Property. In the event that the existing buildings are damaged, demolished or removed, new construction shall maintain a scale and appearance consistent with existing and planned neighboring buildings and grounds. The following standards shall apply:

Standard D-20. Ingress and Egress. Vehicular ingress and egress shall be integrated with that of the adjoining City lot.

Standard D-21. Pedestrian-oriented frontage. Pedestrian-oriented frontage shall be maintained along the east side of the buildings facing the regional bike trail.

Standard D-22. Scale and Appearance. Buildings shall maintain a residential scale and appearance similar to that of the nearby Rice Lane townhomes.

Doherty Park

Policy D-37. Doherty Park Design. Doherty Park should be preserved in its current configuration. Essential features that should be retained include the redwood trees and lawn areas. The existing multipurpose pedestrian/bike path may be improved and realigned within the park.

7.4 SUBAREA 2 DESIGN POLICIES AND STANDARDS

This section provides design policies and standards for Subarea 2. Policies and standards are presented for the Larkspur Plaza property, including the Albertsons grocery store, and the property at the corner of Magnolia Avenue and Doherty Drive that supports a gas station.

Policy D-38. Transitional Downtown Standards. The development standards outlined in Chapter 4, Land Use, and the Transitional Downtown Zoning District regulations (Chapter 18.45 Larkspur Municipal Code) shall apply in Subarea 2. Where the Specific Plan policies and standards differ from the standards in Chapter 18.45, the Specific Plan policies and standards shall supercede.

Larkspur Plaza

The following policies shall apply to any exterior alterations or additions to the Albertsons building.

Policy D-39. Integration with Adjacent Development. Integrate additions or modifications with planned commercial and residential development along the building's west, south and east

edges. Encourage an adjustment to the property lines between the east boundary of Subarea 2 and the west boundary of Subarea 3 to allow for an expanded loading area for the grocery store on the east side of the building and construction of a small commercial or residential building between Subarea 2 and the entry road in Subarea 3. Provide screening and/or landscaping at the backs of buildings and service areas to screen adjacent uses and enhance appearances.

Policy D-40. Pedestrian Linkages. Design pedestrian pathways to encourage walking between Larkspur Plaza and Downtown.

Policy D-41. Adjacent Outdoor Space. Reinforce the attractiveness and vitality of adjacent outdoor space surrounding the railroad buildings.

Policy D-42. Loading Facilities. Protect future residents of the area from noise, unwanted light, and other potential nuisances that may be caused by the service and loading facilities. Approval of any exterior modifications to the existing facility shall be conditioned to require that the developer provide the following at the applicable time:

- A. Concurrent with redevelopment of the contiguous portion of Subarea 3, refurbish the south facade of the existing building so it is more attractive to passers-by and potential neighboring residents.
- B. In the event the existing service dock is relocated from the west to the east side of the building, the following improvements shall be made:
 1. Construct screen along the south and east side of the service area at the east end of the existing building.

2. Provide an interim landscape buffer along the west property edge until such time as exterior alterations or additions are made to this end of the building
 3. Construct a sidewalk with a minimum width of 10 feet along the west side of the building, and provide a direct connection to the east-west pedestrian/bicycle path and the railroad buildings plaza area.
- C. When the existing building is expanded to the west, or when a new building is constructed along the west property edge, the following conditions shall be met:
1. The building should be located to form a well-defined edge to the plaza and the east-west and north-south pedestrian entrances to it.
 2. The southwest corner of the building should be set back to provide for an extension of the railroad plaza area and a south-facing terrace for use by the adjoining commercial space. The outdoor space should match the elevation of the adjoining plaza (approximately 14 feet above sea level).
 3. A direct pedestrian entrance to the interior space should be provided from the plaza.
 4. The southwest building facades should include clear glazed openings to visually link interior and exterior areas and enhance the pedestrian use and enjoyment of the plaza.

The Gas Station Property

Specific Plan land use policies encourage retention of the existing gas station to serve Downtown and nearby neighborhoods. The following policies apply to any changes to the service station or its conversion to other purposes.

Policy D-43. Downtown Entrance. The architecture and landscaping of all structures and open areas shall provide an attractive, well-defined entrance to Downtown.

Standard D-23. Architecture and Landscaping.

Architectural style, building materials, and landscape materials shall complement the architecture and landscaping of the Magnolia Avenue frontage in the Specific Plan area.

Standard D-24. Frontage Landscaping. A well-landscaped buffer shall be provided along both the Magnolia Avenue and Doherty Drive frontages, with building(s) located to define this northern entrance to Downtown.

Standard D-25. Parking. Parking shall be located to the rear of buildings with access and landscaping coordinated with the adjoining Larkspur Plaza parking area.

Policy D-44. Vehicular Ingress and Egress. Existing vehicle ingress and egress should be modified to simplify and clarify traffic movements and minimize conflicting movements at the Magnolia Avenue/Doherty Drive intersection.

Standard D-26. Driveways. Driveway entrances to the gas station property from Magnolia Avenue and Doherty Drive should be eliminated or modified to restrict access to delivery trucks; access to the gas pumps and on-site parking should be provided via the Doherty Drive entrance to Larkspur Plaza

Standard D-27. Drive-in Facilities. Drive-in or drive-through facilities are prohibited so as to avoid impeding vehicular and pedestrian movements. The gas station on Subarea 2 property "F" (see Figure 4-2) is exempt from this standard.

7.5 SUBAREA 3 DESIGN POLICIES AND STANDARDS

Subarea 3 consists of 16.8 acres that have historically been used for the Niven nursery operations. Specific Plan land use policies designate Subarea 3 for residential development and ancillary open space areas. In addition to the many other policies and standards in the Specific Plan, the following section sets design policies and standards for Subarea 3 providing the guiding vision for any development project within Subarea 3. Further, these policies and standards provide the review criteria for the Subarea 3 Planned Development District relative to neighborhood density and design.

Neighborhood Design

Policy D-45A. Larkspur's Values. The overall project proposal shall reflect Larkspur's values (e.g., small town character, walk ability, variety of housing types and styles) and provide the least cumulative impact on traffic, schools, and the environment. In no case shall impacts exceed the impacts identified in the environmental analysis for the Specific Plan.

Policy D-45B. Housing Variety. There shall be a mix of lot sizes and housing types, where no one housing size and type dominates the project. Housing shall include affordable housing and housing that meets the needs of the community's aging population shall be encouraged (e.g., for example, housing with first floor bedrooms and wide doorways).

Policy D-45C. Neighborhood Design. The street layout, residential architecture, and landscaping shall reflect traditional neighborhood design in which homes are designed to relate to the street and sidewalk (e.g., porches projecting in front and garages

setback), and buildings project a human scale creating a sense of neighborhood.

Policy D-46. Residential Architecture. The area shall incorporate residences of varying size (both one and two-story homes) and architecture and maintain a scale and appearance consistent with that of the City's older neighborhoods such as Baltimore Park and Heather Gardens. Diversity in building design shall be provided with a variety of building plans and elevations used to avoid the appearance of a monotonous subdivision. Building height, color, setback, materials, texture, trim and roof shape shall vary throughout the neighborhood.

Policy D-47. East/West Corridor. An open, visual corridor located on an east/west axis toward Mt. Tamalpais shall be provided connecting the north/south reach of Larkspur Creek to Magnolia Avenue. The corridor may consist of physical, visual and/or functional linkages (see Figure 7-2) and should be located along the border with the south edge of Subarea 2 to provide a visual buffer between the commercial buildings and residential uses in Subarea 3.

Policy D-48. Open Space Corridor Design. The open space corridor planned for Larkspur Creek shall be designed to protect water quality, natural habitat, and scenic values. The areas adjacent to the open space buffers may be improved to provide public access and passive use outside of the buffer area if the improvements are designed to be consistent with water quality, natural-habitat protection and scenic values.

Policy D-49. Bicycle and Pedestrian Pathway Design. Planned bike and pedestrian pathways shall be designed to provide convenient and safe connections from new residences in Subarea 3 to the Downtown and to other major destinations, as well as through the property.

Policy D-50. Street Names. Streets in Subarea 3 shall be named for tree species native to the Larkspur area or for flower and plant species that had been grown in the Niven Nursery.

Housing Size and Mix

Policy D-51. Diversity in Size and Appearance, Single Family and Cottage Homes. The siting of single family and cottage homes shall be varied to provide a range of home sizes and a visually diverse appearance on individual street segments and among clusters of homes.

Standard D-28. Lot width and Setback Variations. Vary lot widths and building setbacks for single family and cottage homes to achieve diversity in size and appearance on individual street segments. Figure 7.11 is an illustrative example of this standard.

Standard D-29. Siting Large Single Family Detached Homes. To reduce their apparent size when viewed from the street, limit the width of large single-family homes — those exceeding 3,000 sq.ft. — and/or set their second floors back further from the street than their ground floors. Figure 7-12 is an illustrative example of this standard.

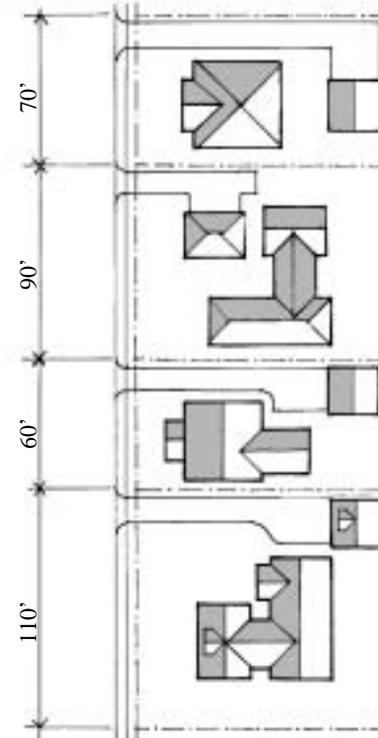
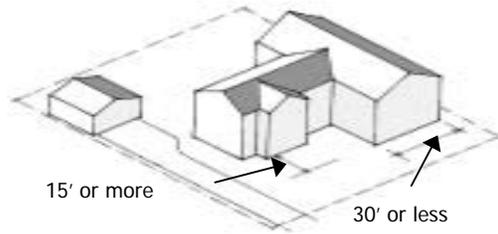


Figure 7-11 Variations in Lot Width and Setback
(distances between adjoining residences average 26 feet)

Break-up massing



Screen two-story structures with one-story structures

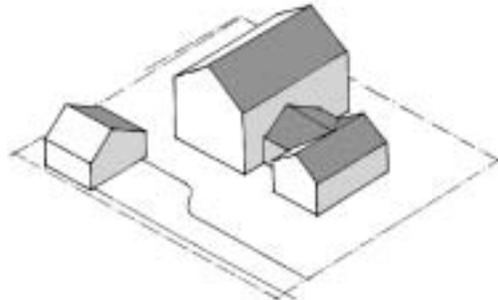


Figure 7-12 Bulk Reduction Guidelines

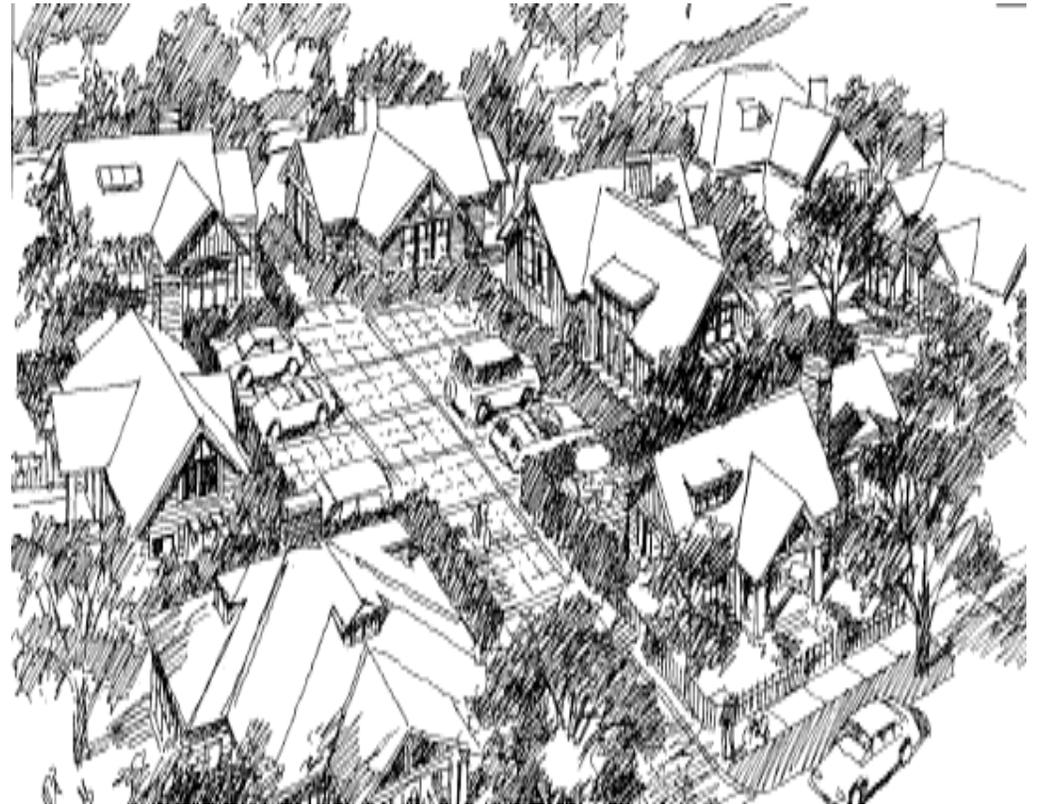


Figure 7-13 Illustrative Only - Cottage Homes

Setbacks and Height

Policy D-52. SINGLE FAMILY HOME Setbacks. The setbacks provided below shall apply to single-family detached residences, except that the standards may be modified through the Planned Development process, if approved by the City Council. Accessory structures shall meet the standards established in the City's Zoning Ordinance for R-1 Zoning Districts, unless otherwise provided within the Plan. Projections into the setbacks shall be allowed consistent with the City's Zoning Ordinance for side and rear yard setbacks and as stated below.

Standard D-31. Front-Yard Setback. A minimum front-yard setback of 20 feet from the property line shall be provided — however, porches, stairs, decks, balconies, bay windows may project up to 5 feet into the front-yard setback.

Standard D-32. Side-Yard Setback. A minimum side-yard setback of 5 feet shall be provided along all interior side yards, and there must be a cumulative side-yard width of 10 feet, or 20 percent of the lot width, whichever is greater. The street side of a corner lot shall have a minimum setback of 10 feet.

Standard D-33. Rear-Yard Setback. A minimum rear-yard setback of 25 feet shall be maintained.

Standard D-34. Detached Garages in Side- or Rear-Yard Setback. Detached garages may be located within a required side- or rear-yard setback, and may be built to the side or rear property line (except on a corner lot, the side yard setback shall be 5 feet), provided the wall on the lot line does not exceed an average height of 10 feet.

Policy D-54. COTTAGE HOME Setbacks. The setbacks provided below shall apply to cottage homes, except that the standards may be modified through the Planned Development process, if approved

by the City Council. Accessory structures shall meet the standards established in the City's Zoning Ordinance for R-1 Zoning Districts, unless otherwise provided within the Plan. Projections into the setbacks shall be allowed consistent with the City's Zoning Ordinance for side and rear yard setbacks and as stated below.

Standard D-36. Front-Yard Setbacks. A front-yard setback of 20 feet shall be provided for street-front lots. However, porches, stairs, decks, balconies, and bay windows may project up to 5 feet into the front-yard setback. Front setbacks for properties that do not have street frontage shall be set by the Preliminary and Precise Development Plans.

Standard D-37. Zero Side-Yard Setbacks. Subject to Design Review approval, residences may abut an interior side yard provided a 10-foot setback is maintained with the building on the adjacent parcel. Accessory structures may abut an interior side yard if approved through Design Review.

Standard D-38. Minimum Side-Yard Setbacks. A minimum 5-foot side-yard setback shall be provided when neither of the adjoining residences is built to the side lot line. The street side of a corner lot shall have a minimum setback of 10 feet.

Standard D-39. Rear-Yard Setback. A minimum distance of 40 feet shall be maintained between residences that back onto each other.

Policy D-55. MULTIFAMILY HOUSING Setbacks and Lot Coverage. The standards provided below shall apply to multi-family housing, except that the standards may be modified through the Planned Development process, if approved by the City Council. Projections into the setbacks shall be allowed consistent with the City's Zoning Ordinance for side and rear yard setbacks and as stated below.

Standard D-40. Multifamily Front-Yard Setback. A front-yard setback of 15 feet shall be provided for street-front lots — however, porches, stairs, decks, balconies, and bay windows may project to within 7 feet of the front property line.

Standard D-41. Multifamily Side-Yard Setback. Each side yard for multifamily dwellings shall have a minimum width of 8 feet. The street side yard of a corner lot shall be 12 feet.

Standard D-42. Multifamily Rear-Yard Setback. The minimum depth of a rear yard shall be 15 feet.

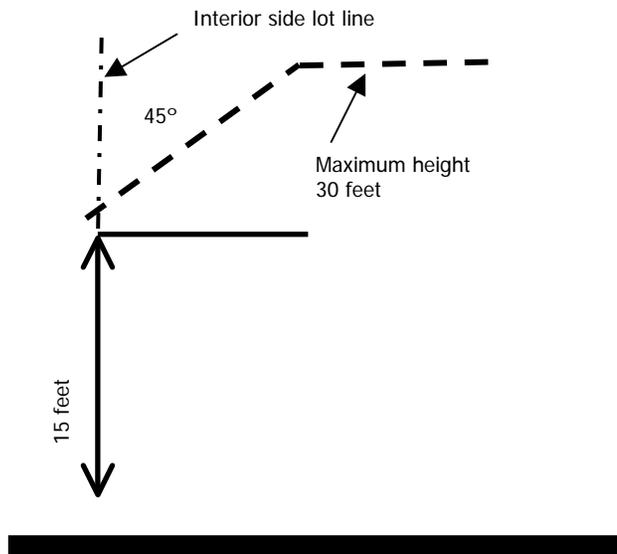


Figure 7-14. Interior Skyplane Standard

Policy D-56. HEIGHT STANDARDS, Single Family and Cottage Homes. The maximum allowable height is 30 feet for single-family homes and 25 feet for cottage homes. The height of cottage homes may be increased if allowed for in Preliminary and Precise Development Plans approved by the City Council.

Standard D-44. Side-Yard Set Back Height Limit. No portion of a structure, except for specifically exempted elements, shall protrude beyond a plane extending at an angle of 45 degrees upward from a height of 15 feet measured from any point on an interior lot line. Figure 7-14 is an illustrative example of this standard.

Standard D-45. Front-Yard Set Back Height Limit. No portion of a structure, except for specifically exempted elements, shall protrude beyond a plane extending at an angle of 45 degrees upward from a height of 20 feet measured from any point on the front-yard setback line. Figure 7-15 is an illustrative example of this standard.

Standard D-46. Skyplane Protrusions. The following protrusions shall be permitted into required side- and front-yard skyplanes.

1. Roof overhangs of 2 feet or less.
2. Architectural elements such as dormers, gables, and walls or railings enclosing an upper-floor balcony or deck, provided that the width of any single element does not exceed 12 feet at the line of intersection with the skyplane, or that the

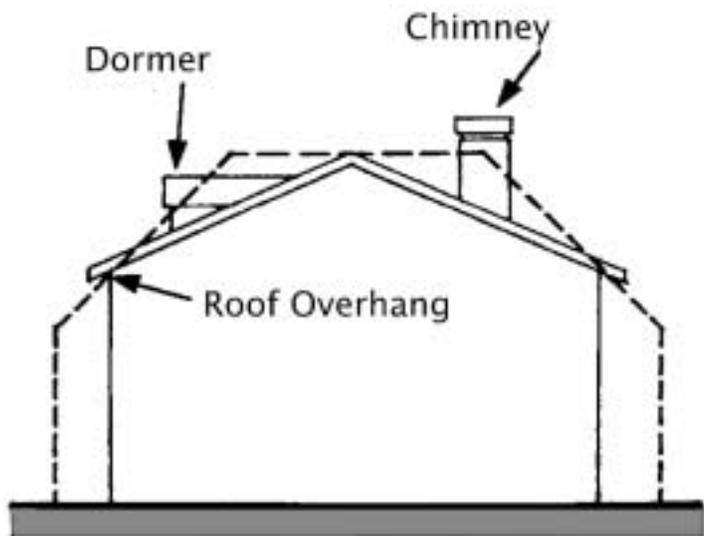


Figure 7.16. Exceptions to Skyplane Standards

combined total width of such protruding elements is less than 35 percent of the width of the related building elevation.

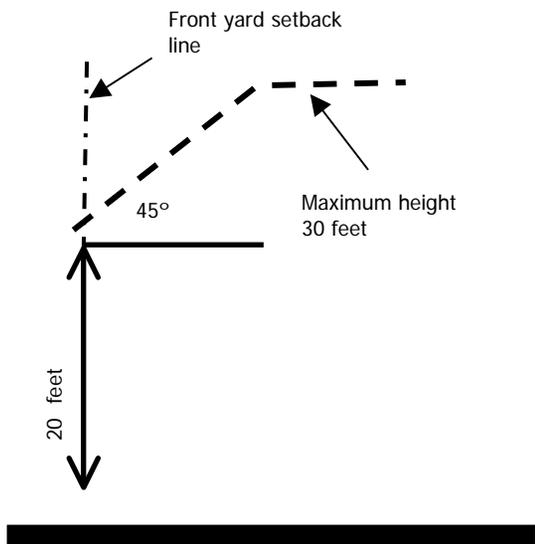


Figure 7-15 Front-Yard Skyplane Standard

Figure 7-16 is an illustrative example of this standard.

Policy D-57. Height Standard, Multifamily Housing. The following shall be the height standards for multifamily housing.

Standard D-47. Height Standard, Multifamily Housing. Multifamily housing shall not exceed 35 feet in height.

Policy D-58. Multifamily Housing, Required Floor Area. The following shall be the minimum floor area standard for multifamily housing.

Standard D-48. Minimum Floor Area, Multifamily Housing. Each dwelling unit in a multifamily building shall have a minimum floor area of 450 square feet.

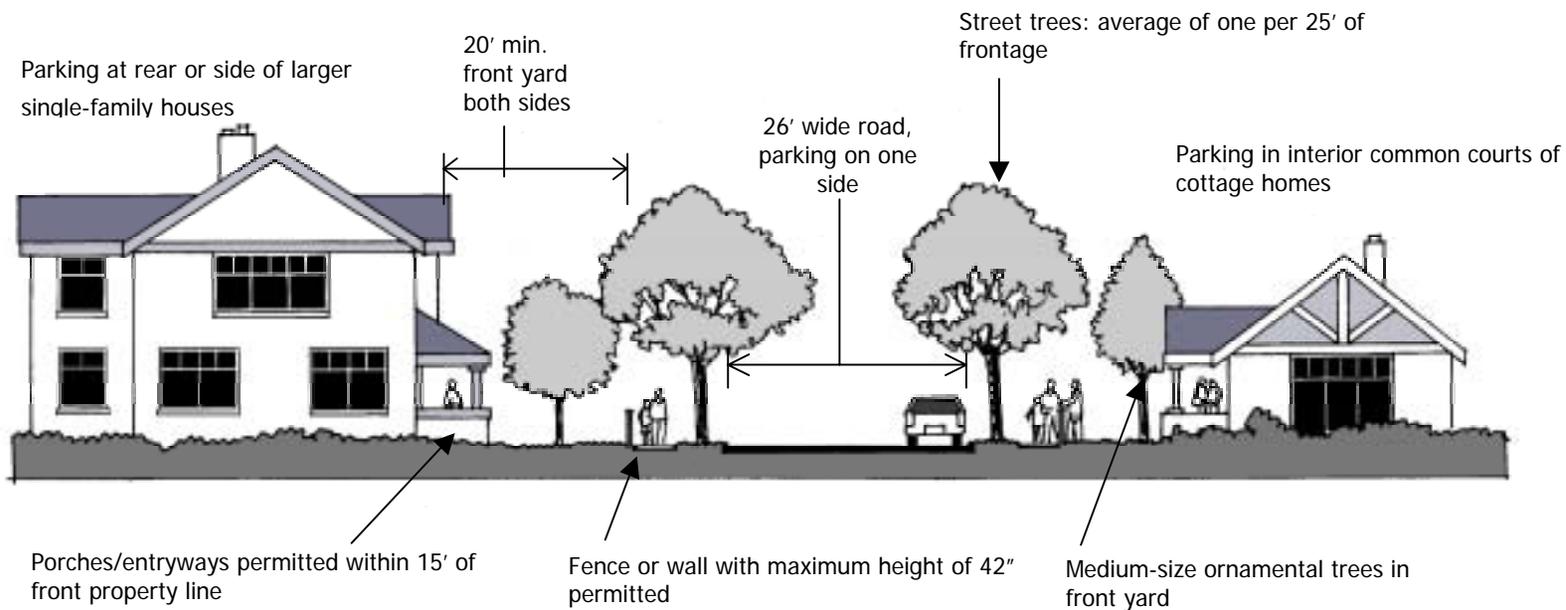


Figure 7-17 Summary of Streetscape Standards and Guidelines

Streetscape

Policy D-59. Streetscape Standards. The following standards apply to street rights-of-way and adjoining front yards. Figure 7-17 is an illustrative example of these standards.

Standard D-49. Street Tree Placement. A street tree shall be planted for every 25 feet of lot street frontage. These trees shall be planted within a 5-foot planting strip adjoining each side of the roadbed of a residential street or within the first 5 feet of the edge of the sidewalk.

Standard D-50. Tree Species to be Limited. Two to four species of trees should be used and planted within the curbside-planting strip.

Standard D-51, Deciduous Trees. Predominant use should be made of deciduous trees to provide shade in summer and allow for sunlight during the winter.

Standard D-52. Tree canopy. Use round-headed and/or fanned-shaped trees to eventually create a conjoined tree canopy along the street. Tall, columnar and pyramidal form trees may be used to accentuate intersections, major entrances, and pedestrian/bicycle crossings.

Standard D-53. Street Tree Species. Street tree species should include, but not be limited to, the following:

Round-headed or fan-shaped

- ❖ London Plane (*Platanus acerifolia*)
- ❖ Red Maple (*Acer rubrum*)
- ❖ Western Sycamore (*Platanus racemosa*)
- ❖ California Live Oak (*Quercus agrifolia*)
- ❖ Common Hackberry (*Celtis occidentalis*)

Columnar or Pyramidal

- ❖ Bradford Pear (*Pyrus Calleryana*)
- ❖ Oregon Ash (*Fraxinus latifolia*)
- ❖ European Hornbeam (*Carpinus betulus*).

Standard D-54. Shared Driveways. Use of shared driveways serving two to four single-family homes is allowed as a way to reduce curb cuts and provide greater visual diversity.

Standard D-55. Front-Yard Fence Height Limits. Front-yard walls, fences and hedges shall not exceed a height of 42

inches and should use materials, textures and colors that are consistent with, or complementary to, the associated residence.

(Note: Fencing for side and rear yards shall be consistent with the City's Zoning Ordinance fence regulations.)

Standard D-56. Street Lights. Street light posts shall not exceed a height of 14 feet. Posts should be spaced approximately 60 feet apart on alternating sides of the street. Cutoff lenses should be used to prevent light from spilling over onto facing residences.

Doherty Drive Frontage

Alignment and intersection requirements for Doherty Drive are set forth in Chapter 5. Applicable design standards follow.

Policy D-60. Doherty Drive Frontage Standards. The following standards shall apply to designated sections of the Doherty Drive right-of-way and/or to designated properties fronting on the right-of-way.

Standard D-57. Noise Mitigation. Habitable residential space shall be located a minimum distance of 70 feet from the south curb face of Doherty Drive. A combination of garages and masonry, stucco, or architecturally finished concrete walls shall be used to reduce roadway noise for adjoining residences. Additional noise analysis shall be done at the time of submittal of the tentative map, and supplementary noise-abatement methods shall be required, if needed.

Standard D-58. Existing Liquidambar Trees. Best efforts shall be made to retain the existing Liquidambar trees on the Doherty Street frontage adjacent to Subarea 3 to create the

sense of a parkway containing a Class 1 bikeway; there should be flexibility in the right-of-way design to allow putting the bike path to the south of the trees, if necessary to retain the trees. (See also Standard T-1 A.) Additional liquidambar trees should be planted to supplement the existing trees and complement the existing formal landscape. If removal of the trees is necessary, they should be replaced with comparable landscaping.

Standard D-59. Street Trees At Hall Middle School. A uniformly spaced row of liquidambar trees should be planted along the north side of Doherty Drive adjacent to the south edge of the Hall Middle School site to mirror the trees on the south side of Doherty Drive.

Standard D-60. Subarea 3 Entry Intersections. Special design treatment shall be provided for the two Doherty Drive intersections that will serve as entrances to Subarea 3. Special pavement and landscape features shall be used to make the pedestrian and bicycle crossings readily evident to motorists. These and other design features should also highlight the

entrance to the new housing areas. Figure 7.18 is an illustrative example of this standard.

Pedestrian/Bicycle Circulation

Chapter 5 establishes a system of pedestrian paths and bikeways to serve both future Plan-area residents and the community at large. More detailed standards are set forth below for various segments.

Policy D-61. Pedestrian Path and Bikeway Standards. The following standards shall be applied to the design of pedestrian paths and bikeways.

Standard D-61. Class 1 Bikeways. The minimum paved width of a Class 1 bikeway shall be 9.5 feet. A minimum 2-foot horizontal clearance shall be maintained to obstructions adjacent to the pavement. A 12-foot width is preferred when a bikeway is to be used by both pedestrians and bicycles, or when use by service vehicles is anticipated. Bikeways may be surfaced with asphalt.

Distinctive paving to distinguish pedestrian and bicycle crossings. *This is not a traffic circle.*

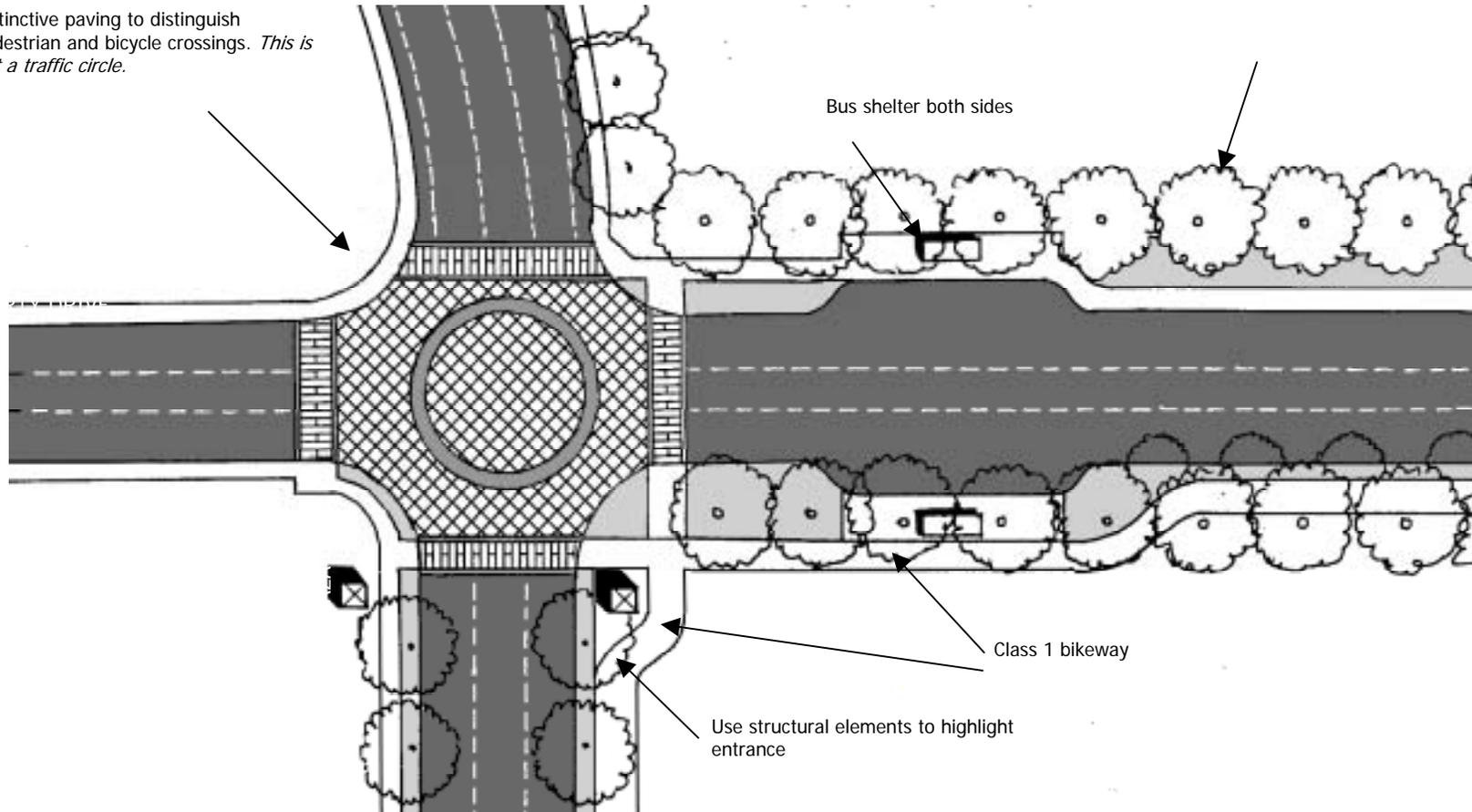


Figure 7-18 Illustrative Only- Plan for Doherty Drive/Larkspur Plaza Drive Intersection Showing Recommended Design Treatments

Liquidamber trees
25' on center

Policy D-62. Pedestrian Walkways. The pathway links required by transportation policies and standards (Chapter 5) may be provided in one or more of the following ways.

- ❖ They may be incorporated into a residential street right-of-way.
- ❖ They may pass through a common open space, such as that in a cottage home development.

- ❖ They may be located within a separate right-of-way with a minimum width of 9.5 feet.

Standard D-62. Walkway Widths. The paved surface of walkways within greenways shall be a minimum of 5 feet wide.

Policy D-63. Traffic Calming Designs. The policies and standards set forth in Chapter 5 are intended to ensure the safety of pedestrians. Specifically, the limits on street width and avoidance of through-routes will reduce vehicle speed and avoid intrusion by unrelated traffic. Additional traffic-calming methods may also be incorporated into the design of residential streets within Subarea 3 to further promote a pedestrian-friendly environment. Figures 7-19, 7-20, and 7-21 illustrate appropriate methods.

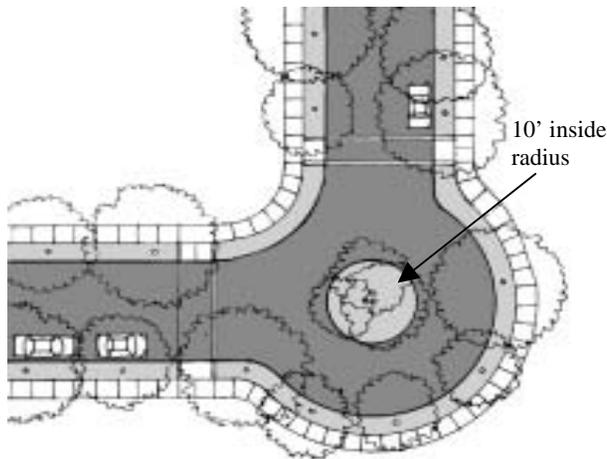


Figure 7-19 Illustrative Only –
Traffic Calming: Circle

Natural Resource Protection

Many aspects of natural and scenic resource protection pertaining to Subarea 3 have been addressed in the preceding standards and guidelines. Among these are the following: protection of Larkspur Creek; reduced road widths to minimize impervious surfaces; requirements for street trees; skyplane height standards that help ensure solar access; and use of pervious surfaces for pedestrian paths, bikeways, and parking areas. The following are several additional targeted provisions.

Policy D-64. Habitat Enhancement. The applicant should undertake a cooperative effort with the City of Larkspur and local environmental interests to enhance the natural habitat value of Larkspur Creek and adjoining protected open space. Possible enhancement actions include regrading of the northern portion of the creek to expand the wetland area.

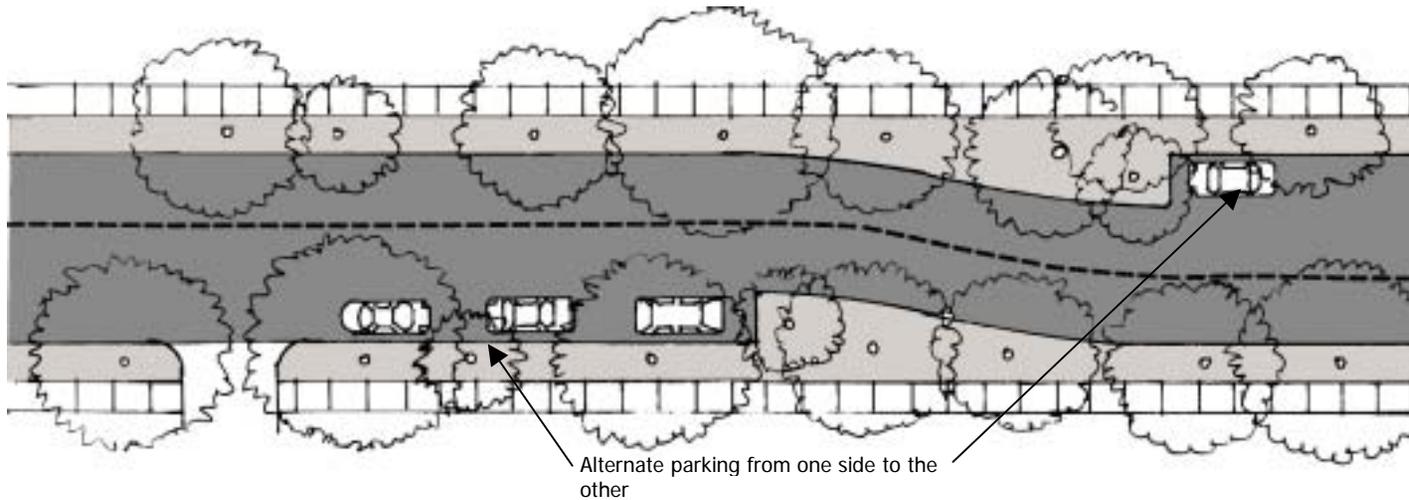


Figure 7-20 Illustrative Only –
Traffic Calming Street jog

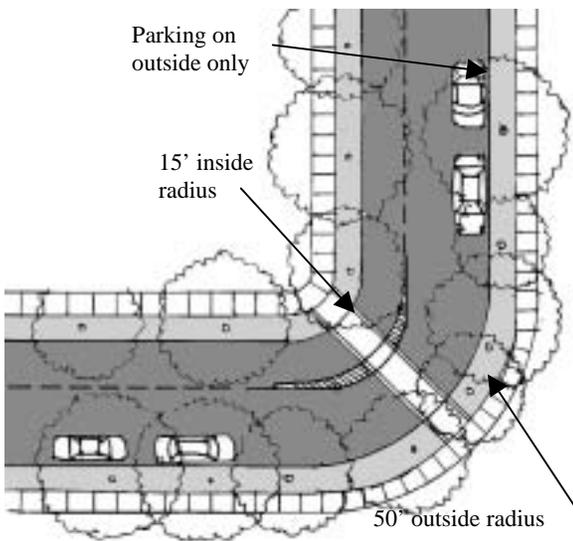


Figure 7-21 Illustrative Only –
Traffic Calming: Elbow

Standard D-62A. Native Restoration Plan. The developer of Subarea 3 shall prepare, and the City shall review and approve, a native plant restoration plan for upland habitat for the Larkspur Creek buffer area. The restoration plan shall be developed by a qualified restoration ecologist, and shall include the following components: proposed methods to eliminate non-native, invasive species; a native plant planting and irrigation plan that considers and is compatible with any water treatment and stormwater detention ponds; a description of a proposed monitoring schedule; and performance standards to ensure that the restoration effort is successful. Target species for removal shall include French and Spanish broom, oleander, Himalayan blackberry, pampas or jubata grass, and fennel. Recommended replacement species include, but are not limited to, arroyo and Pacific willow, coyote bush, native bunchgrasses, toyon, and coast live oak. Implementation of the native plant restoration plan shall be a condition of any project approvals in Subarea 3. Monitoring reports prepared by a qualified restoration ecologist

shall be submitted to the City annually for 5 years. The first report shall be due to the City 12 months following the start of implementation of the restoration plan.

Standard D-62B. Signage and Interpretative Displays.

Less than 12 months following the start of implementation of the restoration plan, signage that includes interpretive displays shall be posted on bikeways and footpaths alerting visitors to the nearby sensitive habitat and explaining the importance of protection of these areas. Signs shall also be posted requiring that all dogs be on leashes and kept out of the setback area.

Policy D-65. Natural Resource Protection Standards. The following standards shall be enforced to protect natural resources in the Plan area:

Standard D-63. Creek Setback. Building setbacks and natural resource buffers shall be maintained along Larkspur Creek for the purposes of water quality and natural-habitat protection as well as to assist in providing visual and noise buffers between the Subarea 3 residential development and the Tamalpais Union High School District corporation yard and Tamiscal High School. The setback and buffer standards are provided below:

- 63 A. The natural resource buffer shall be at least 50 feet wide from the top of bank on the north/south reach of the creek at the eastern edge of the Specific Plan area. The buffer area shall be fenced to prevent human and/or pet intrusions. No building or impervious surface may be located closer than 50 feet to the top of bank.
- 63 B. The natural resource buffer shall be at least 25 feet wide from the top of bank on the east/west reach of

the creek at the southern edge of the Specific Plan area. The buffer area shall be fenced to prevent human and/or pet intrusions. No building or impervious surface may be located closer than 50 feet to the top of bank.

Standard D-64. East Entrance to Subarea 3. The east entrance to Subarea 3 from Doherty Drive shall be aligned with the entrance to Piper Park, and the road shall be aligned along the open space adjoining Larkspur Creek for a distance of approximately 500 feet.

Standard D-65. Creek Setback and Buffer Ownership. Land lying within the creek setback and buffer shall be retained in common ownership to ensure appropriate management of the habitat and watercourse buffer. The CC&Rs for development within Subarea 3 shall include controls relating to pesticide use.

Policy D-66. Heritage Trees. A study shall be conducted to identify potential heritage trees located along the western edge of Subarea 3, adjoining the former railroad right-of-way. Heritage trees of the species listed in Standard D-66 shall be retained and incorporated into the design of redeveloped areas.

Standard D-66. Trees To Be Retained. The following trees shall be retained and incorporated into the design of redeveloped areas:

- ❖ The Oregon White *Oak (Quercus garryana)* located immediately southeast of the warming house.
- ❖ The Valley/White *Oak (Quercus lobata)* located approximately 80 feet south of the warming house.

- ❖ The small grove of redwood trees (*Sequoia sempervirens*) between the warming hut and East Ward Street.
- ❖ The Valley/White Oak located in the southwest corner of the Niven property adjoining East Ward Street.

Policy D-67. Existing Ornamental Plants. Ornamental trees and shrubs located throughout the subarea (such as the stand of large camellias in the southwest corner) should be retained and incorporated into new landscaping when this is feasible based on their condition and location, or on the possibility of moving them.

Policy D-68. Energy-Efficient Residential Design. Residences should be located and designed to maximize opportunities for passive solar heating in winter, prevention of undesirable heat gain in summer, and natural lighting year round. The following standards should be applied in addition to standard construction techniques.

Standard D-67. Detached Garages. Detached garages are encouraged to maximize the possibility for natural lighting of interior spaces and passive solar heating.

Standard D-68. Roof Overhangs. Provide sufficient roof overhangs to shade south-facing windows during mid-summer. Other design elements that are generally effective in this regard are awnings, canopies, and well-positioned deciduous trees.

Standard D-69. Clerestory Windows. Use clerestory windows or skylights to light large interior areas where the orientation of a lot makes it difficult to provide natural light using conventional fenestration. When properly designed, clerestory windows may also be used to direct heat into north rooms in the winter and provide natural ventilation and cooling in the summer.

Policy D-69. Storm water. The following standards should be incorporated into the siting of houses to minimize the flow of stormwater off-site and allow bio-filtering of pollutants.

Standard D-70. Driveways and Parking Areas. Driveways and common parking areas should be constructed of permeable surfaces such as turf block or brick, asphalt or concrete pavers set in a sand bed.

Standard D-71. Grassy Swales. Open space landscape design should incorporate grass swales to provide interim ponding and treatment of first-flush runoff, which carries a high concentration of oils and other pollutants. The open space buffer along Larkspur Creek, the common open space around the cottage homes, and the park areas in Subarea 3 all provide opportunities for such swales.

Standard D-72. Stormwater Detention. Retention/detention ponds for holding and gradually releasing stormwater should be incorporated into the open space system of Subarea 3.

Standard D-73. Native Plants. Native grasses and other native plants should be used in areas that filter or detain stormwater or provide for protection or enhancement of natural habitats.

Standard D-74. Paving Foot Path Surfacing. Within Subarea 3, all footpaths located outside of a street right-of-way, shall be surfaced with pervious materials.

Policy D-70. Air Quality-Fire Places. Residential wood burning appliances and fireplaces shall be prohibited. Only natural gas or pellet burning fireplace appliances shall be permitted as a condition of approval of all planned development permits for residential

construction. This measure effectively eliminates more than 90% of emissions, thus mitigating emissions below the level of significance. Natural gas and pellet residential heating stove emissions are almost inhalable particulate matter (PM10) free; thus, wood smoke impacts would be eliminated. This measure also controls PM10 emissions and avoids contributing to existing violations of the 24-hour and annual PM10 standards. Carbon monoxide (CO) and toxic air contaminants (TACs) from combustion would also be almost completely eliminated by this measure.